

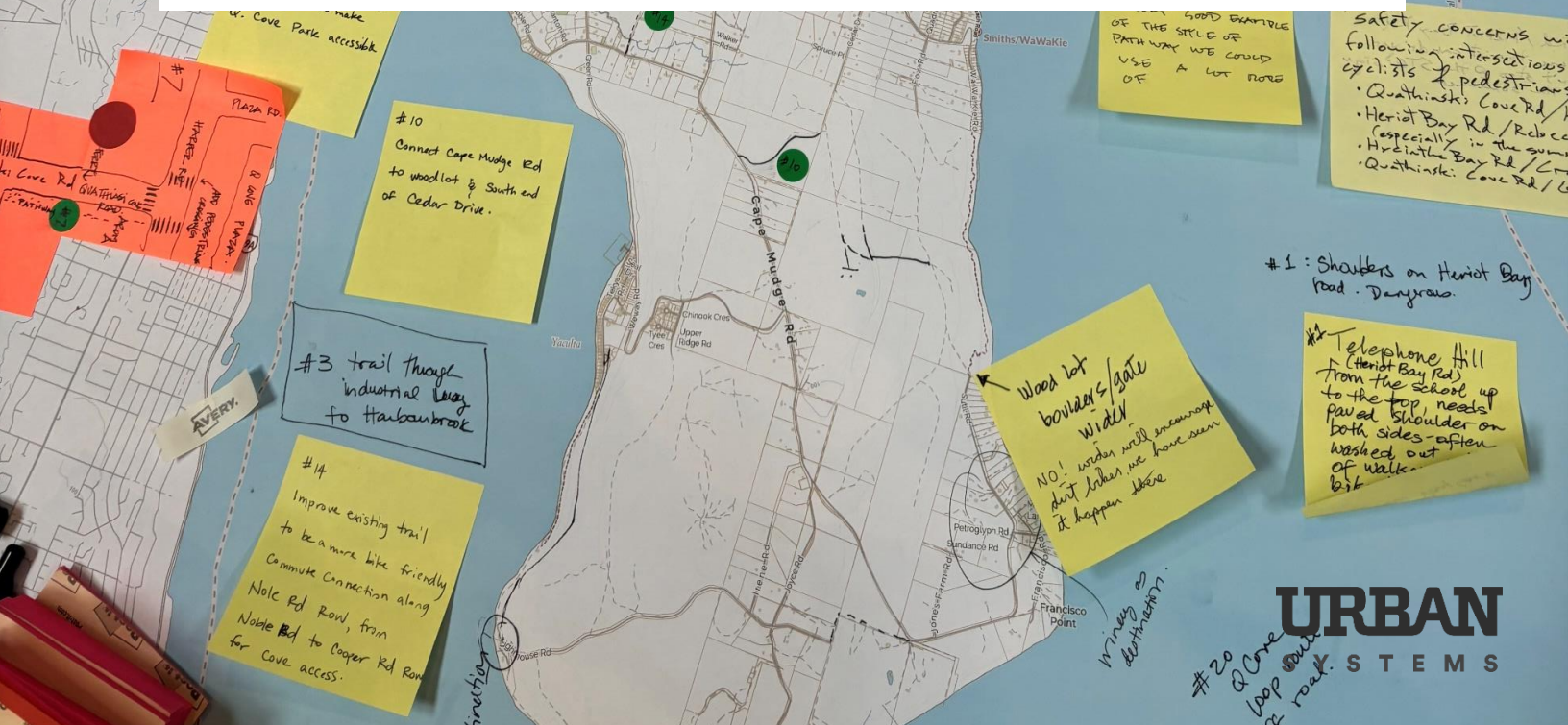


Strathcona Regional District  
Electoral Area C

# Active Transportation Network Plan

## Phase One Engagement Summary

December 2024



## Introduction

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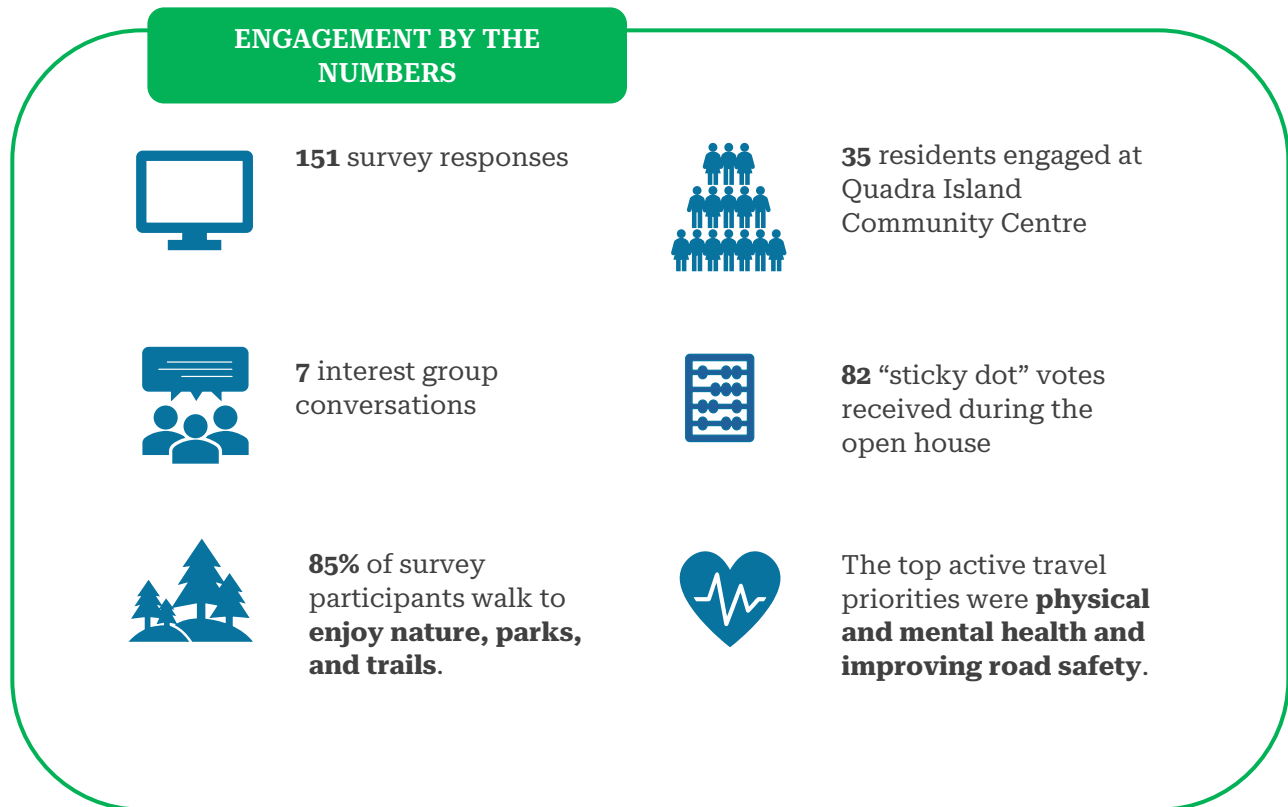
The activities undertaken as part of this initial phase of engagement for the Strathcona Regional District (SRD) Electoral Area C (“Area C”) Active Transportation Network Plan (ATNP) support the following engagement objectives:

- To inform community members of the purpose and potential outcomes of the Area C ATNP
- To engage residents and key interest groups in dialogue on active transportation challenges and opportunities, collect ideas to generally improve the active transportation network, and integrate priorities within existing and planned networks
- To educate community members of the benefits of active transportation and accessibility
- To collect and incorporate meaningful feedback to ensure the ATNP is reflective of community needs and priorities, and to ensure concerns and ideas are reflected in the proposed network and priorities

## How We Engaged

Public and interest group activities were undertaken during October and November 2024 to gather input from Area C residents and understand key challenges related to active transportation in our community. Engagement activities included:

- Community Survey: Open from October 15<sup>th</sup> to November 15<sup>th</sup>
- In-person open house at Quadra Island Community Centre: October 29<sup>th</sup>
- Conversations with key interest groups and local First Nations: Throughout November



## Key Takeaways

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A list of key takeaways from community and interest group engagement activities undertaken to date has been summarized below and will help inform the Draft and Final ATNP.

### Barriers to Walking and Cycling

Residents and visitors identified a number of barriers to cycling and walking throughout Quadra Island, including a lack of dedicated active transportation facilities and roadside shoulders, narrow lanes, lack of sidewalks, as well as vehicle traffic issues such as speed, volume and noise.

### Lack of Connectivity

Area C residents expressed a desire to see improved active transportation connectivity between key Quadra Island neighbourhoods and commercial cores (Quathiaski Cove and Heriot Bay), as well as improved trail conditions.

### Origins and Destinations

Quathiaski Cove Ferry Terminal is a key trip generator for those traveling to and from Campbell River. Residents of Cortes Island also regularly utilize the Heriot Bay Ferry Terminal to travel through Quadra Island to reach Campbell River. Safe and efficient active transportation connections to both ferry terminals were identified as critical for the community. Additional community destinations of importance for active transportation connections include Quadra Elementary School, Quadra Community Centre, Blenkin Park and Cape Mudge.

### Road Safety

Area C residents expressed a general concern over safety for active transportation users on roads due to high vehicle speeds, blind corners, steep topography and narrow travel lanes without adequate roadside shoulders or dedicated active transportation infrastructure.

# What We Heard

## Community Survey

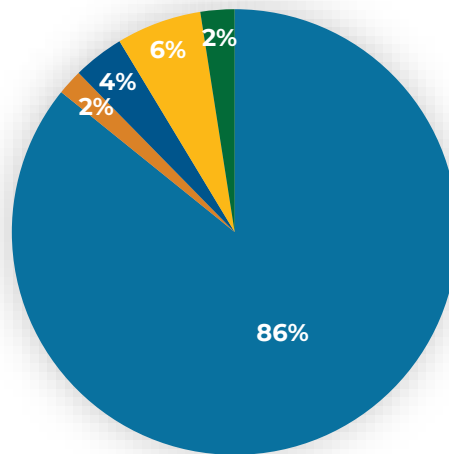
To better understand current travel habits in Area C and other priorities, the SRD hosted a project survey on its webpage from October 15<sup>th</sup> to November 15<sup>th</sup>, 2024. Paper surveys were also made available at the Tru Value Foods in Quathiaski Cove during this period. The survey was supplemented by a mapping exercise which allowed participants to indicate and comment on specific locations of concern or priority in the community.

Overall, there were **151 survey responses** submitted to the Regional District. The following section is a summary of what we heard through the online survey.

### About You

1. Which best describes you?

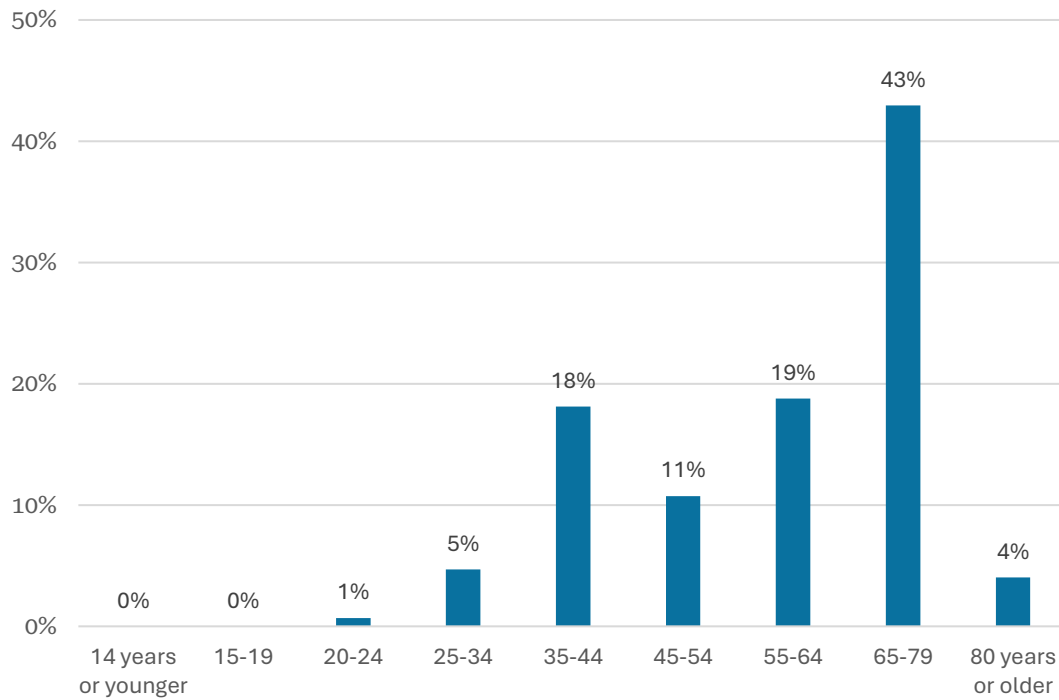
Over 85% of respondents were residents of Quadra Island. Another 6% regularly travel through Quadra Island to reach another community, likely Cortes Island.



- I am a permanent resident of Quadra Island
- I am a resident of another community in Area C
- I am a resident of another electoral area in the SRD
- I am a business owner, or I work in Area C
- I regularly travel through Quadra Island to reach another community
- I visit Quadra Island often
- I am a member of the We Wai Kai First Nation
- I am a member of another Nation with territory in Area C

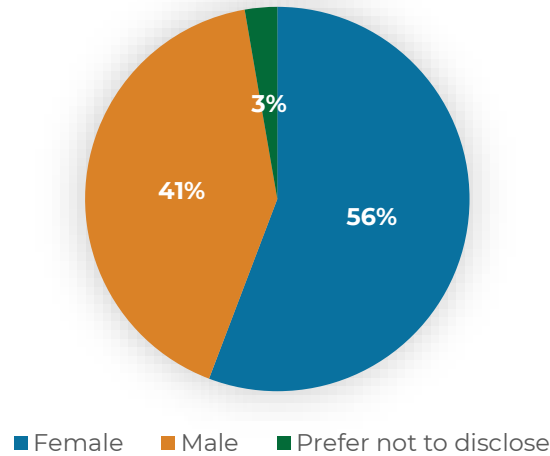
## 2. What is your age?

The highest percentage of respondents (43%) were between the ages of 65 and 79 years old. Approximately 19% were between the ages of 55 and 64. The results suggests that while Quadra Island has a significant senior population that is interested in enhanced active transportation connections, other age groups may be underrepresented in the survey results.



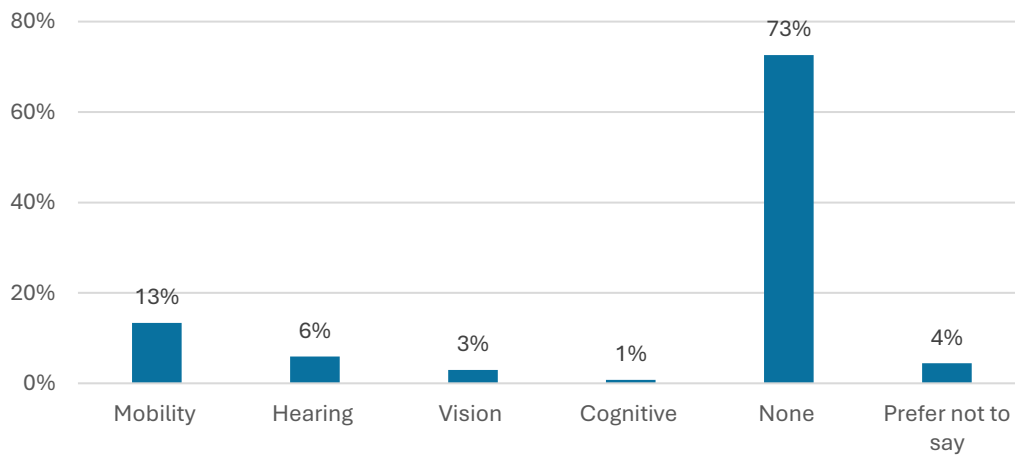
### 3. What is your gender?

Over half of all respondents (56%) were female, 41% were male, and the remaining 3% preferred not to disclose their gender.



### 4. Do you have any limitations?

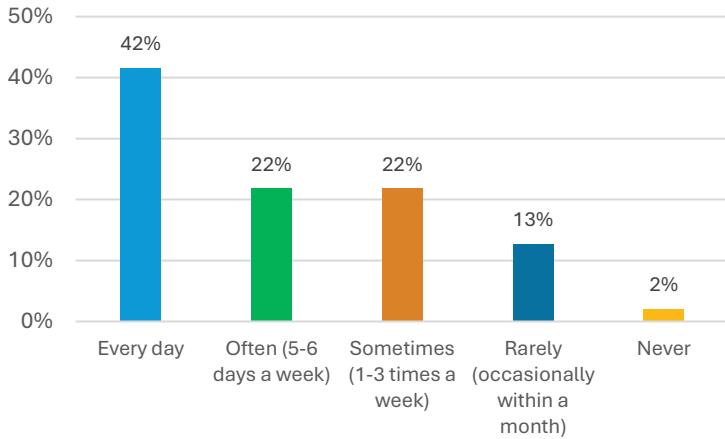
While approximately 73% of respondents do not have any limitations, 23% have some sort of limitation, all of which will be taken into account when planning accessible active transportation design for all ages and abilities.



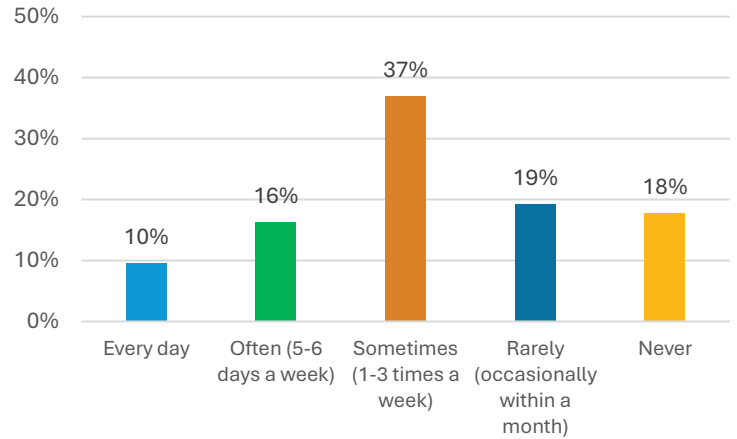
## Travel Patterns

### 5. In a typical week, how often do you use the following modes of transportation?

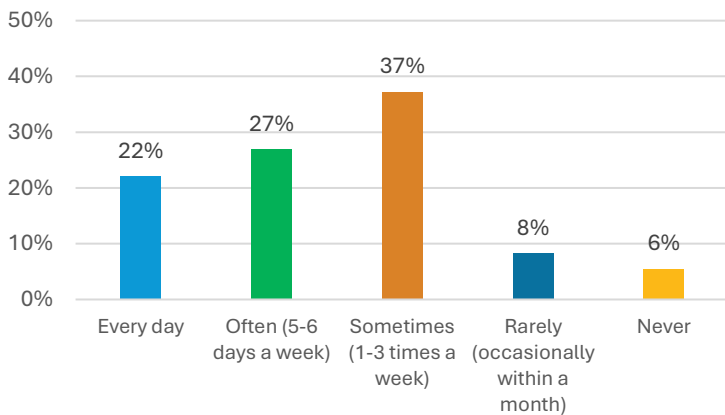
Walking



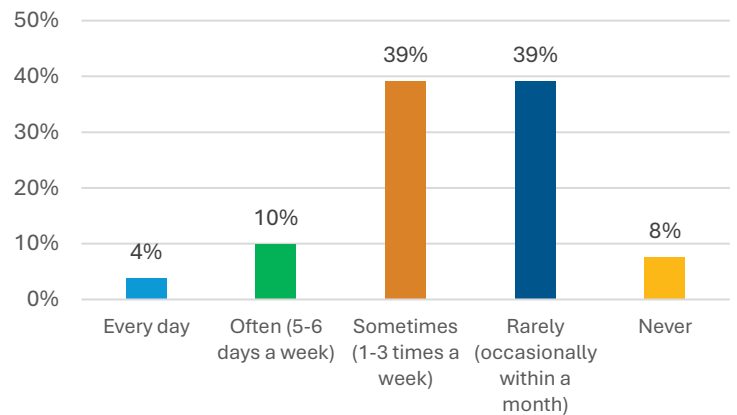
Cycling



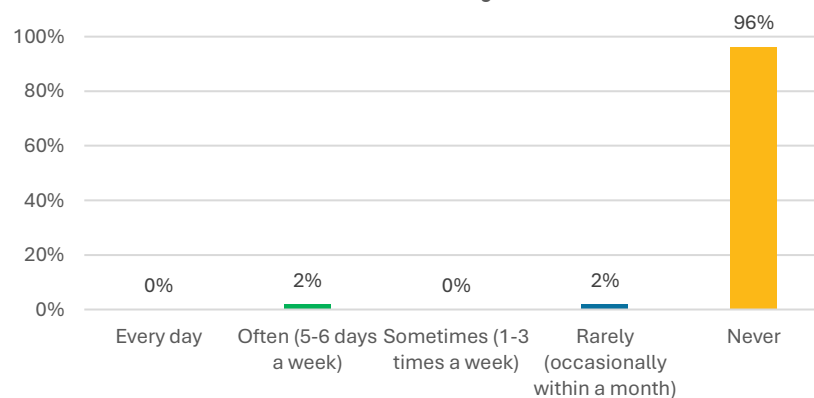
Driving



Passenger in a Vehicle

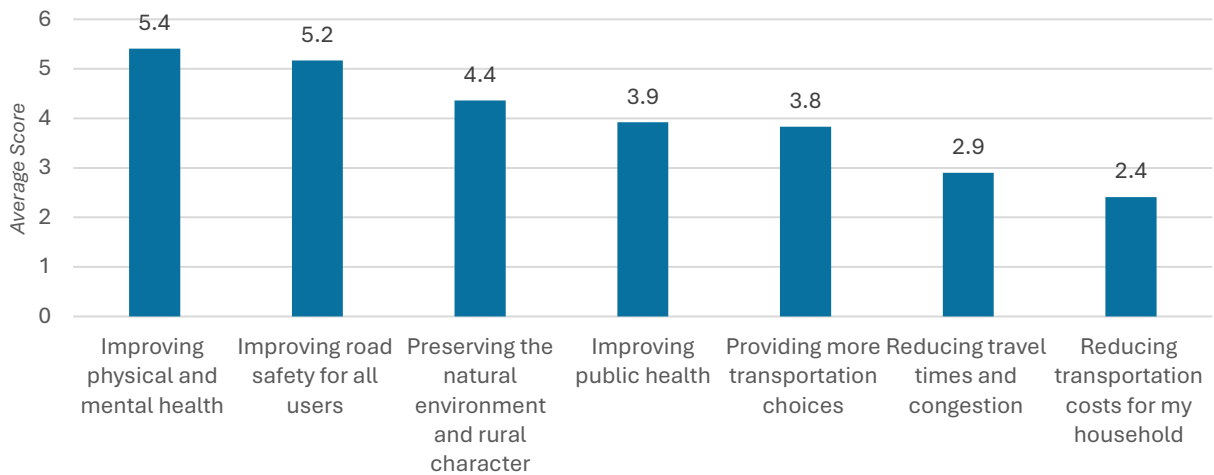


Use of a Mobility Device



**6. Please rank the following active transportation priorities in order of importance to you. (1 = Highest priority)**

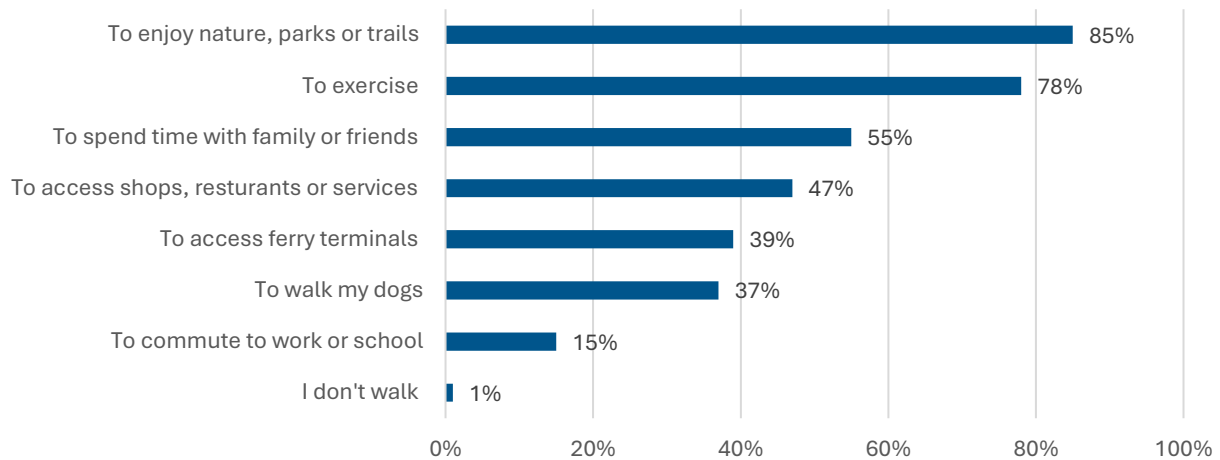
Survey respondents indicated their top priority was “improving physical and mental health,” followed by “improving road safety for all users” and “preserving the natural environment and rural character.”



**Walking**

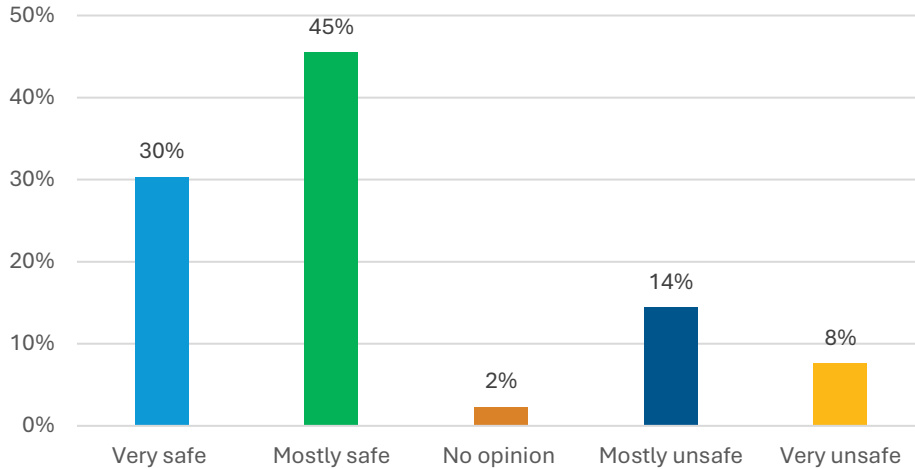
**7. When I walk, it is to:**

Most survey respondents indicated they walk for recreational purposes, such as enjoying nature (85%) and for exercise (78%). Only 15% walk to commute to work or school.



8. How safe do you feel walking in Area C on a typical day?

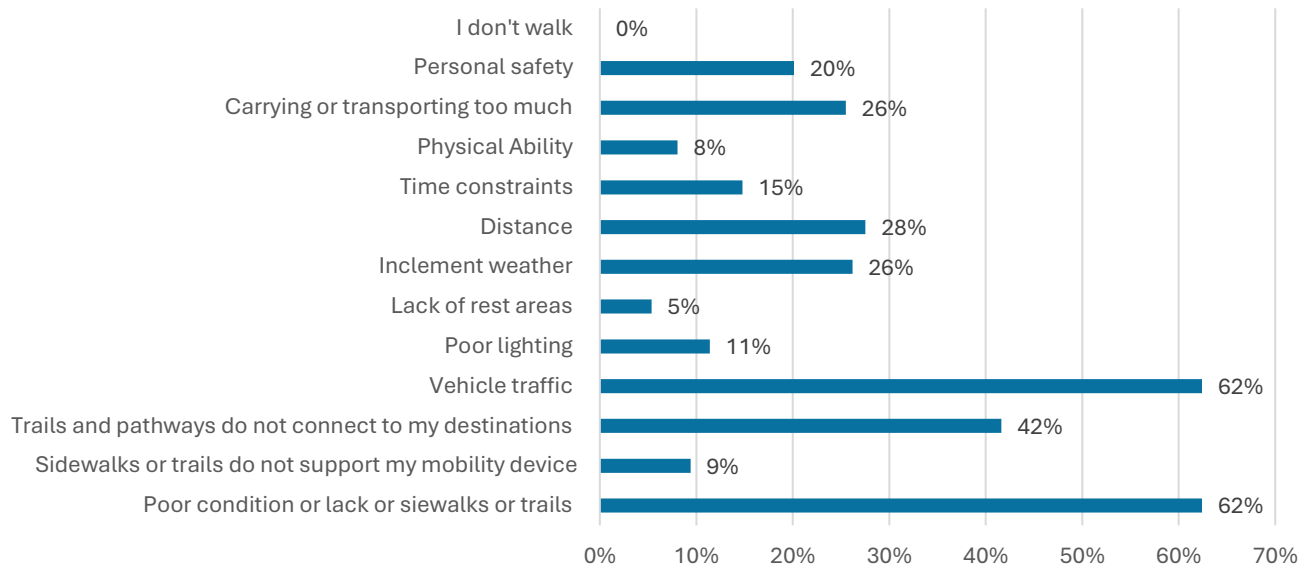
75% of respondents indicated that they feel very safe or mostly safe while walking in Area C. Another almost 23% indicated they feel mostly or very unsafe walking.



“Other” responses indicated that their perception of safety depends on the location, time of day and level of traffic on various streets. Residents of Quadra Island feel unsafe walking alongside roadways due to the lack of shoulders, while off-road trails feel much safer.

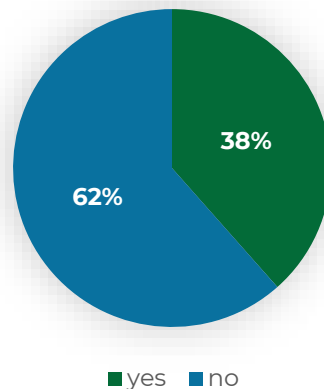
9. What are the main barriers or challenges for walking (including walking by wheelchair, motorized scooter, assistive device) in Area C?

Out of 149 responses to this question, the most common challenges experienced by respondents included the poor condition or lack of sidewalks and trails, speed and proximity to traffic and lack of trail and pathway connections to commercial destinations and neighborhoods.



10. Do you face barriers walking on Quadra Island and its connections to the outer communities?

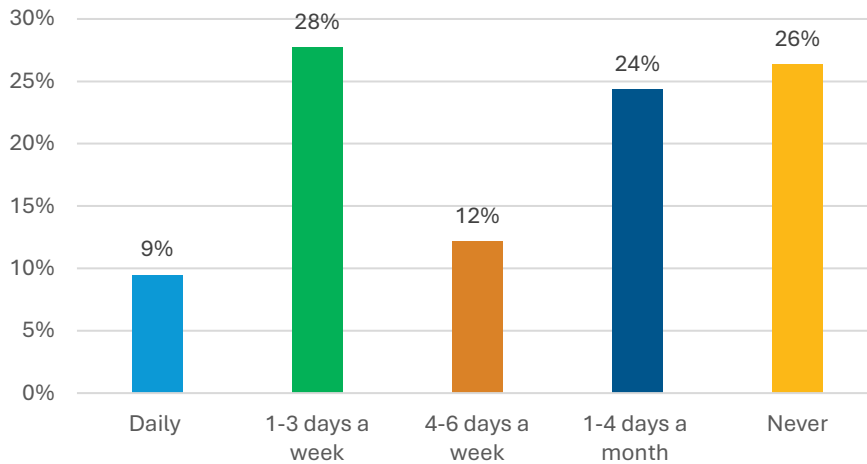
While more than 62% of respondents do not face barriers walking on Quadra Island, another 38% indicated they do face barriers.



## Cycling

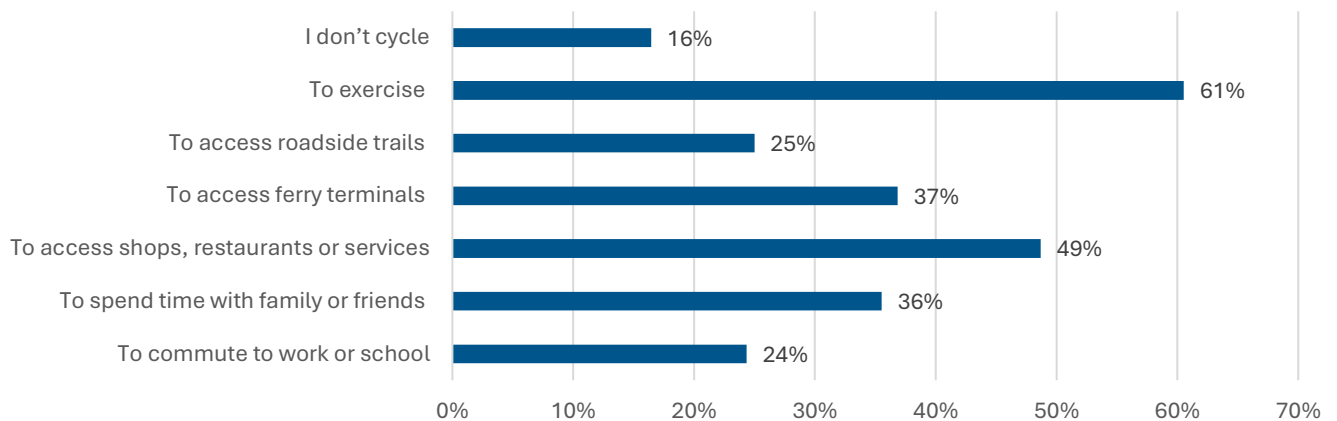
### 11. In a typical month, how many times do you ride a bicycle?

Approximately 28% of respondents ride a bicycle 1-3 days a week, and another 24% cycle around 1-4 days a month. 26% indicated they never cycle.



### 12. When I cycle, it is to:

More than half of total respondents (61%) cycle for exercise and 48% of respondents indicated they cycle to access shops, services and restaurants. Another 37% of respondents cycle to access ferry terminals.

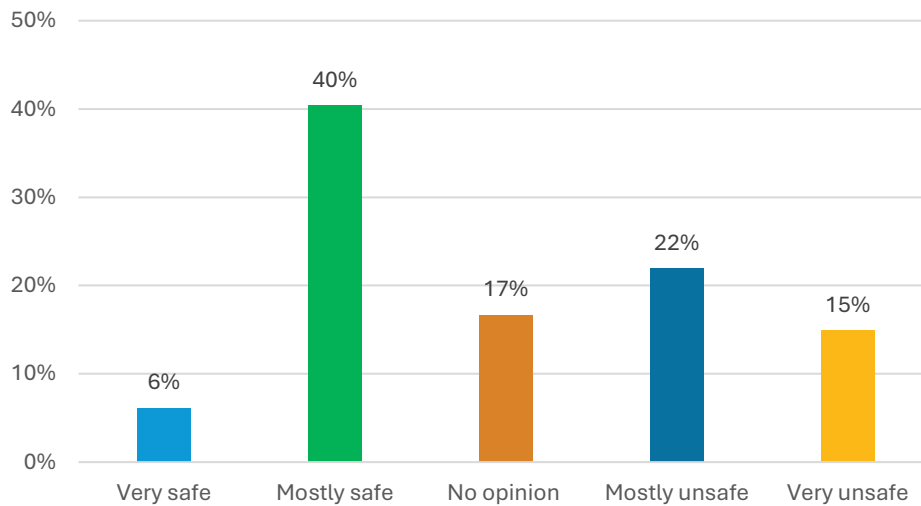


### 13. How safe do you feel cycling on Quadra Island and the outer communities on a typical day?

Approximately 46% of respondents feel very or mostly safe while cycling, and 37% feel very or mostly unsafe.

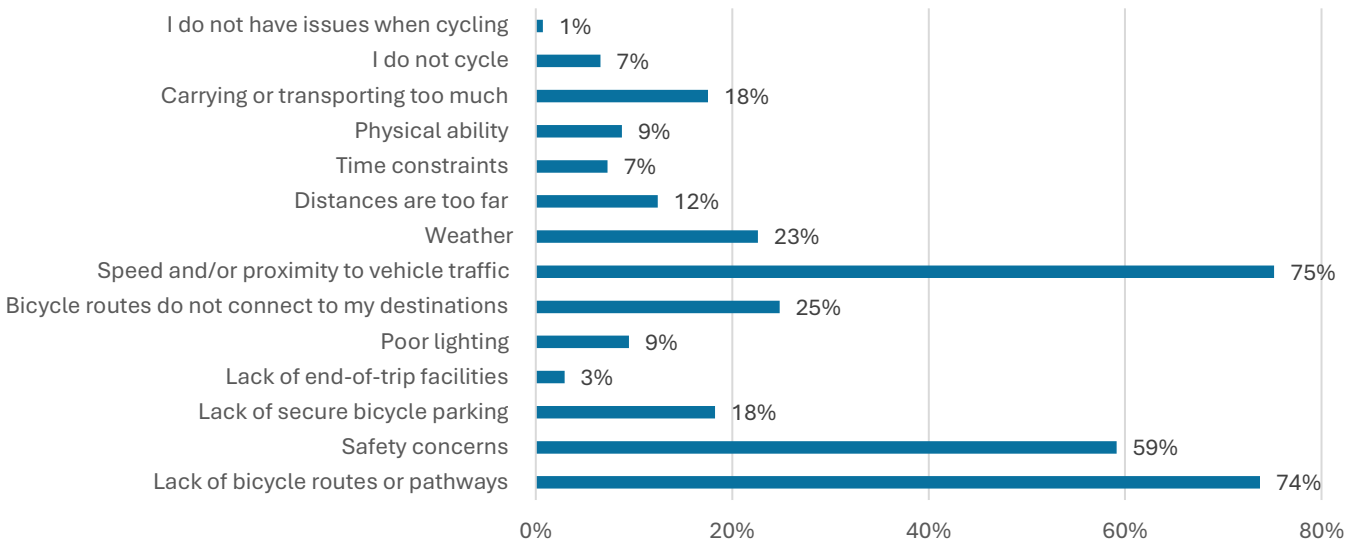
“Other” responses included the following:

- Road shoulders are too narrow
- Blind corners make some spots very dangerous
- Speeding vehicles rarely move over for cyclists and make cycling very unsafe



**14. What are the main barriers for cycling on Quadra Island and its connections to the outer communities?**

Of the 137 responses, the most common barriers for cycling included “speed/proximity to motor vehicle traffic,” followed by “lack of bicycle routes or pathways” and “safety concerns.” Other barriers included “bicycle routes not connecting to desired destinations”, “weather” and “lack of secure bicycle parking.”



**15. If you would like to share more information on the barriers you experience while cycling, please indicate.**

Out of 65 responses to this question, the most common themes and ideas that emerged are outlined below:

- Large vehicles do not provide enough passing space next to cyclists
- Speeding vehicles and blind corners make cycling scary
- Lack of shoulders/narrow shoulders in some areas
- Lack of covered bicycle parking
- Unsafe conditions for students walking and cycling near the school
- Desire for dedicated bike lanes and/or wider roadside shoulders

16. Please use this space to indicate specific locations where you have concerns related to cycling facilities:

There were 64 responses to this open-ended question. Locations of cycling concern are summarized below, and generally correspond with locations mentioned in the mapping exercise:

- Heriot Bay Road
- West Road
- Quathiaski Cove Road
- Harper Road
- Rebecca Spit Road

## Locations of Concern (Mapping Exercise)

The image on next page presents locations on the map as colors based on density of comments and community concerns. These locations/areas will be used in subsequent stages of the ATNP to understand priority areas for improvement.

Areas with a high density of community active transportation comments as per Figure 1 are indicated by the purple color, and include the following locations:

### **Quathiaski Cove Village**

- No crosswalks for pedestrians or cyclists
- Ferry traffic and unloading speeds create issues for pedestrians and cyclists trying to cross Harper Road to the Cove
- Lack of bicycle racks at ferry terminal and Village

### **Quadra Elementary School**

- No sidewalks and accessible shoulders around school
- Crosswalks are in poor conditon

### **West Road at Heriot Bay Road, Heriot Bay Ferry Onramp**

- Traffic congestion from Cortes Island ferry traffic
- Lack of cyclist and pedestrian infrastructure

### **Heriot Bay Road between Rebecca Spit and West Road**

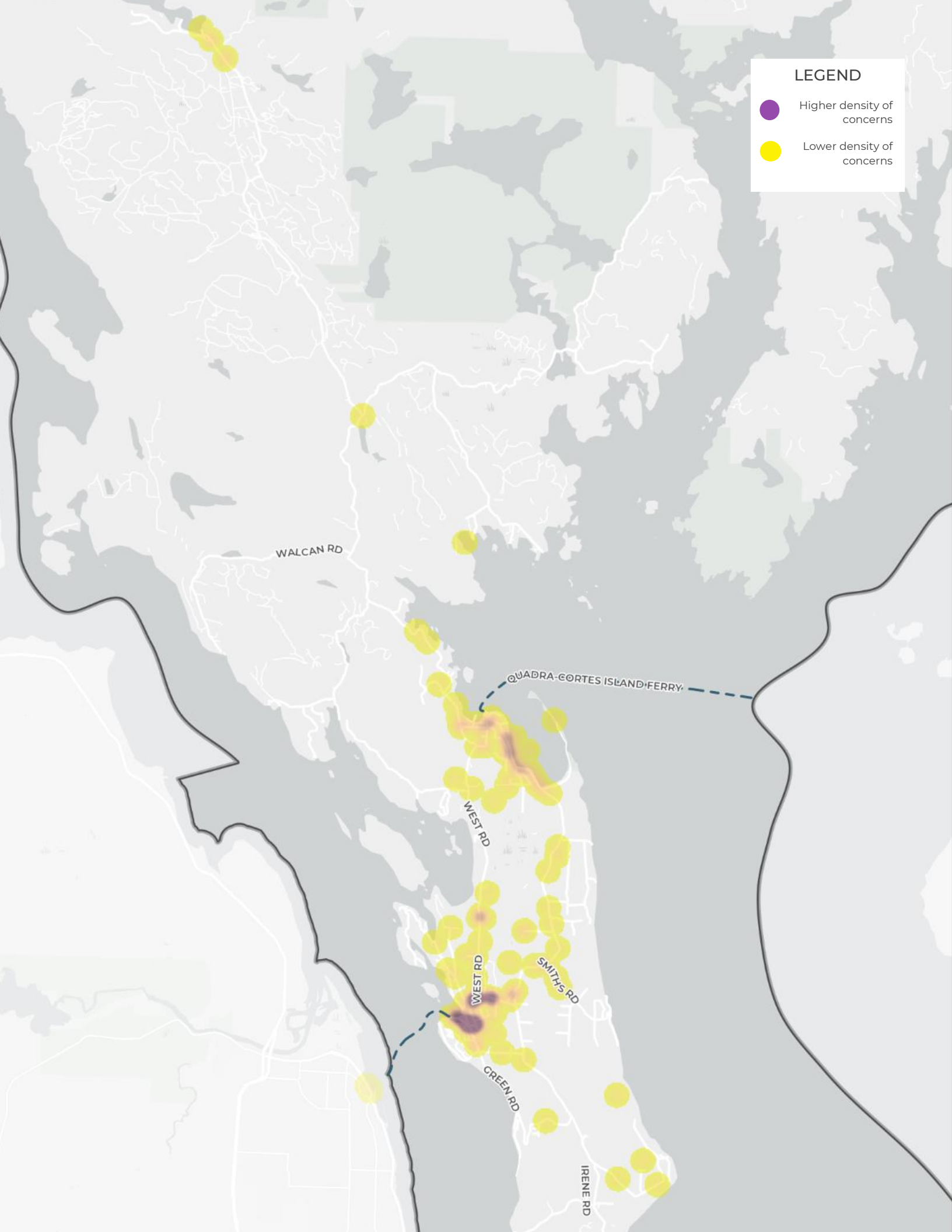
- Blind corners, narrow lanes and steep hills create safety issues for pedestrians and cyclists

### **West Road**

- Desire to see a roadside trail or accessible shoulder connecting the community center to Quathiaski Cove and the elementary school

### LEGEND

- Higher density of concerns
- Lower density of concerns



## Open House

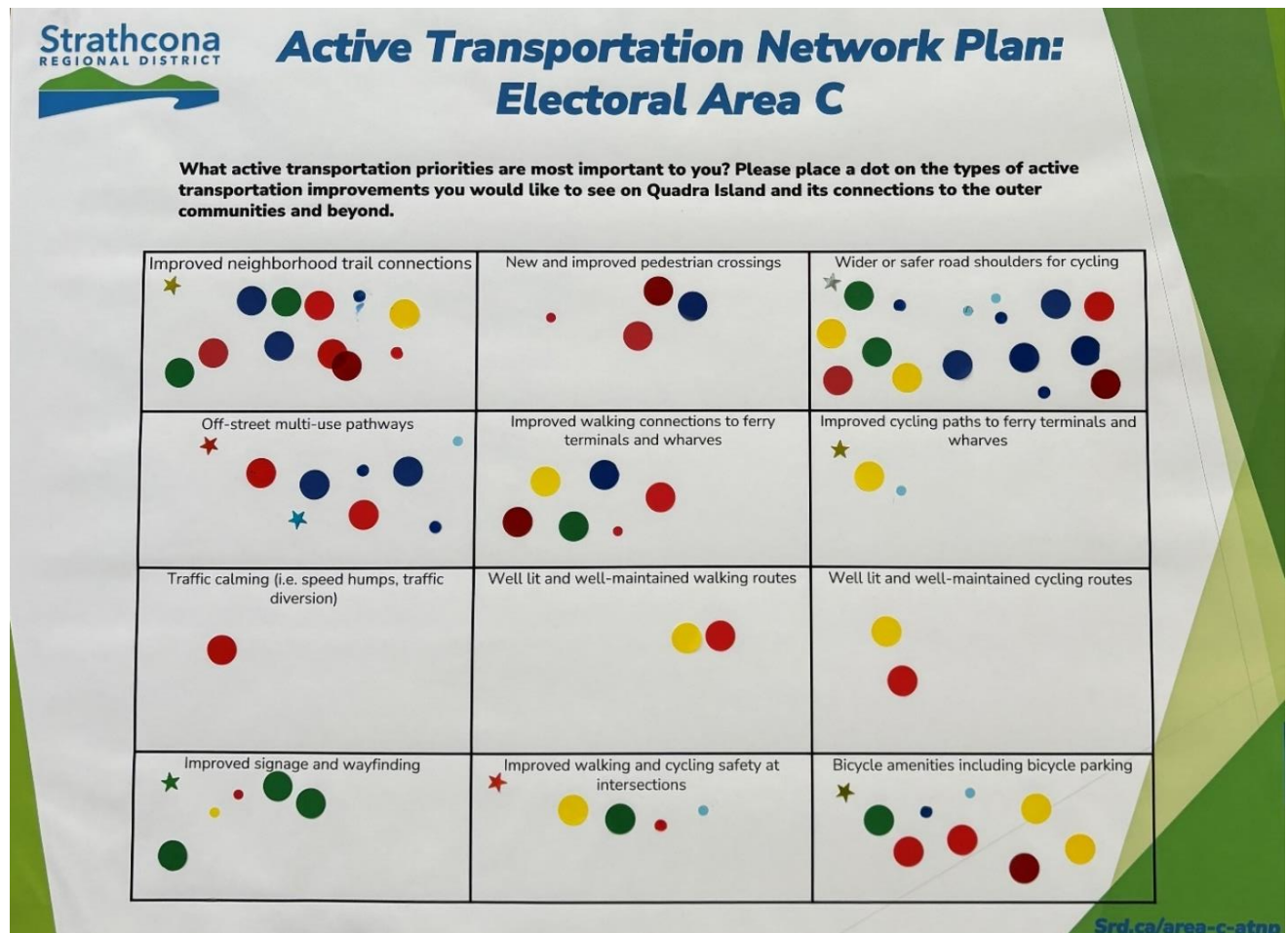
On October 29<sup>th</sup>, 2024, the project team hosted an open house at Quadra Island Community Centre to promote the ATNP and invite community members to participate in a collaborative session. 35 residents were in attendance, many of them active members of the Quadra Island cycling coalition and active transportation users.

A facilitated mapping activity allowed participants to indicate locations of specific concerns related to active transportation and safety on Quadra Island.

The “dotmocracy” activity invited participants to place a sticker-dot on the active transportation priorities and facility types that are most important to them.

As shown below on the dotmocracy board, participants indicated they would most like to see:

- **Wider and safer road shoulders** (17 votes)
- **Improved neighborhood trail connections** (12 votes)
- **Bicycle amenities including bicycle parking** (9 votes)
- **Off-street multi-use pathways** (9 votes)





## Key Interest Group Conversations

During the month of November, the project team met with key interest groups to discuss how the Area C ATNP might address existing priorities and concerns from community organizations. The following interest groups were spoken with during this round of engagement:

- Quadra Island Trails Committee
- Quadra Island Foundation
- Quadra Community Centre
- Quadra Island Cycling Coalition
- Quadra Circle Community Connections Society
- Surge Narrows Community Association
- BC Ferries

Key themes that emerged from these conversations, relevant to the ATNP, included:

- Emphasis should be on coordination and consistent design of active transportation facilities between key destinations and ferry terminals, in coordination with BC Ferries.
- SRD should work in collaboration with community organizations such as Quadra Island Cycling Coalition and the Quadra Island Trails Committee who are directly involved with volunteer-led pathway and trail maintenance.
- An aging population on Quadra Island necessitates active transportation infrastructure that is accessible for seniors and people with disabilities.
- Challenges exist with current road widths putting active transportation users directly adjacent traffic.
- There is a desire to work with the Ministry of Transportation & Transit (MOTT) to realize wider shoulders and/or roadside trails.
- Opportunities exist to create new off-road trails to make key connections between key origins and destinations.
- The success of the Harper Road multi-use pathway is a case study for what successful future active transportation facilities could look and feel like.

The project team also met with the We Wai Kai First Nation to discuss Nation input into the Area C ATNP development. Some comments included a lack of accessible shoulders, sidewalks or roadside pathways and an interest in walkable pathways, perhaps along Green Road, to support a safer, walkable connection between Quathiaski Cove and Cape Mudge.

## Next Steps

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Input and feedback received through community engagement activities will be used in the following next steps:

1. A framework will be developed that outlines the vision, goals and objectives of the ATNP.
2. Preliminary network plans will be developed that reflect directions, engagement takeaways and the planning framework.
3. A second round of community engagement is planned for the Winter of 2025, where community input will be sought on the draft ATNP.
4. Final ATNP will be presented to the Regional Board in March 2025
5. A shovel ready project will be identified by March 2025



EXIT

QUADRA ISLAND NORTH  
Regional District of Strathcona

- places you would like to see better access to ferry terminals
- places you would like to see safety improvements for pedestrians and cyclists
- places you would like to see improved trail connectors
- places you would like to see not limited to:
- improvements may include (but are not limited to):
- adding and/or placing a street on locations and
- improving streets for walking, cycling and/or comments!

us where you would like to  
 adding and/or placing a  
 street on locations and  
 improving streets for walking,  
 cycling and/or comments!



Strathcona  
Regional District